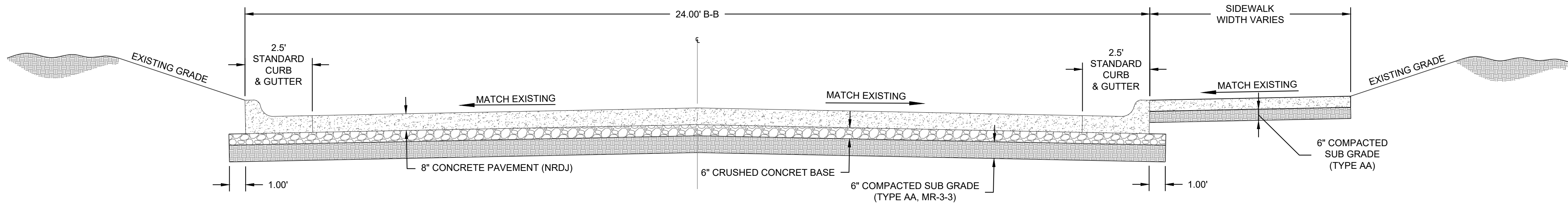
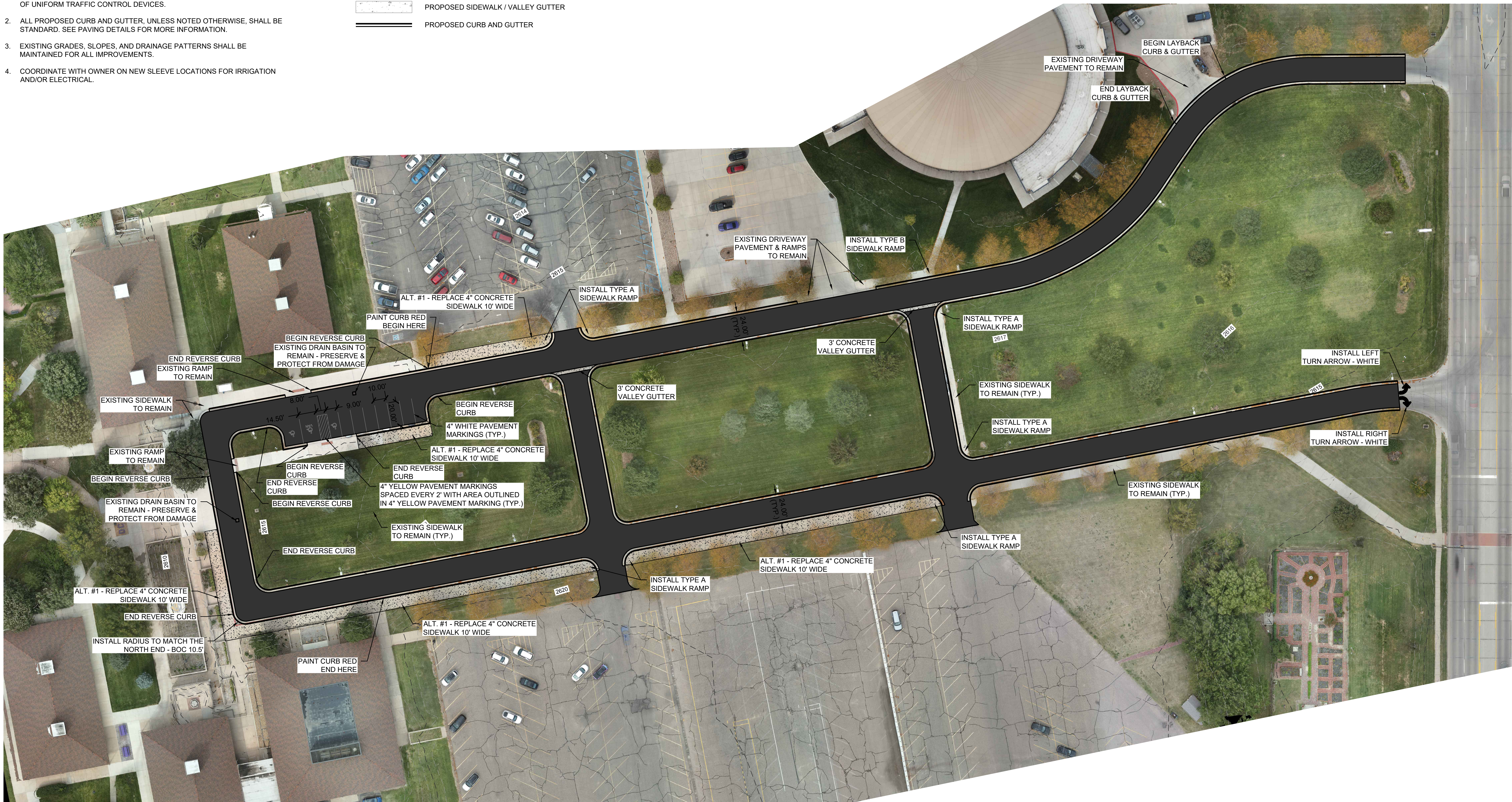


NOTES:

1. ALL TRAFFIC CONTROL DEVICES AND PAVEMENT MARKINGS, SYMBOLS, AND STRIPING SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
2. ALL PROPOSED CURB AND GUTTER, UNLESS NOTED OTHERWISE, SHALL BE STANDARD. SEE PAVING DETAILS FOR MORE INFORMATION.
3. EXISTING GRADES, SLOPES, AND DRAINAGE PATTERNS SHALL BE MAINTAINED FOR ALL IMPROVEMENTS.
4. COORDINATE WITH OWNER ON NEW SLEEVE LOCATIONS FOR IRRIGATION AND/OR ELECTRICAL.

LEGEND

- PROPOSED CONCRETE PAVING
- PROPOSED SIDEWALK / VALLEY GUTTER
- PROPOSED CURB AND GUTTER



\*\*MATCH EXISTING GRADES, SLOPES, & DRAINAGE PATTERNS\*\*

BURKE DRIVE TYPICAL SECTION  
NOT TO SCALE

DODGE CITY COMMUNITY COLLEGE

BURKE DRIVE REPLACEMENT DOCUMENTS

DODGE CITY, KANSAS

SITE PLAN & TYPICAL SECTION

REVISION DESCRIPTION  
(DESCRIPTION)

REVISION DATE

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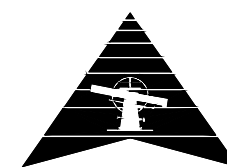
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NORTH



40' 20' 0'  
SCALE: 1" = 40'

PROJECT #: 2508-0324  
CHECKED BY: KML  
DRAWN BY: KML

DATE: 11/07/2025

SHEET #

1

TOTAL SHEETS

5

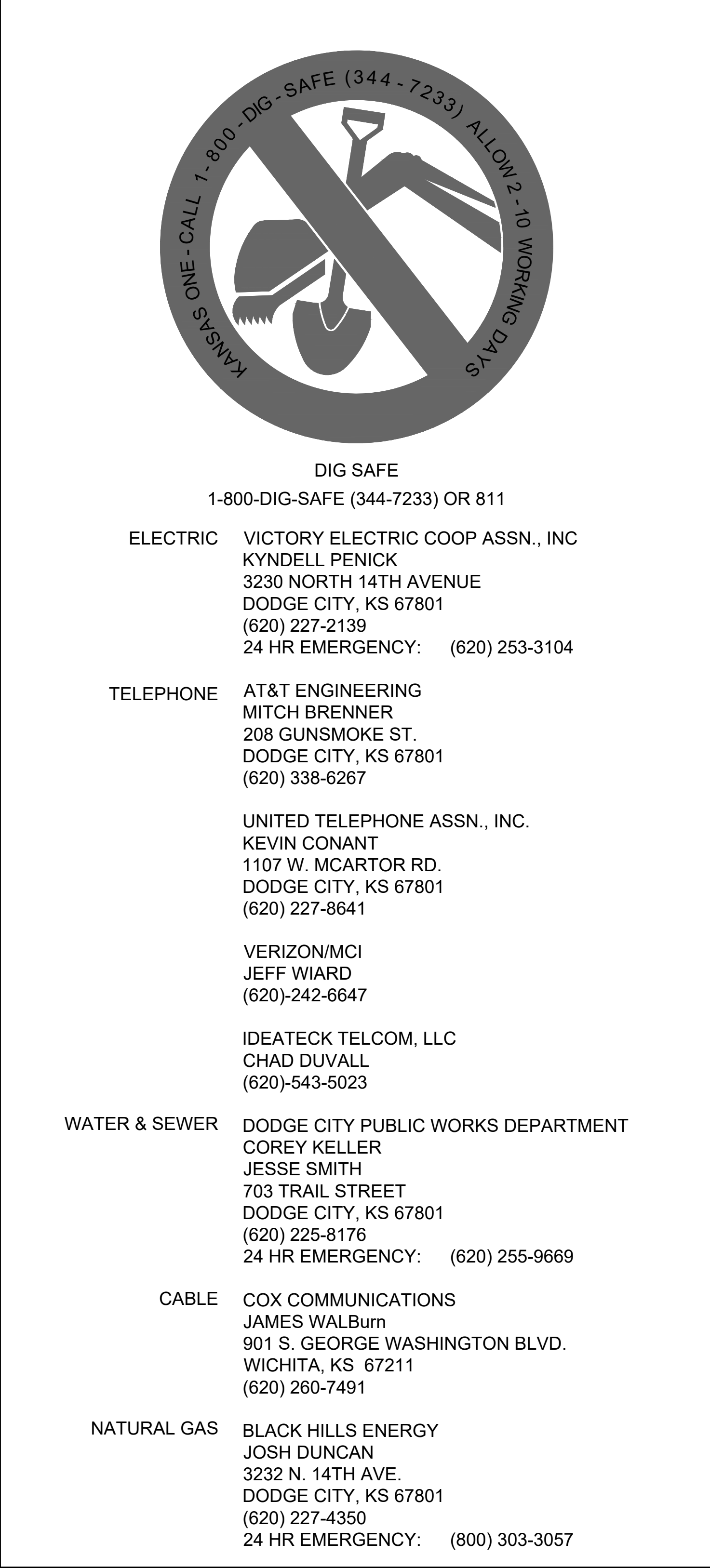


## GENERAL NOTES

1. THE MEANS OF THE WORK AND THE SAFETY OF THE CONTRACTOR'S EMPLOYEES ARE SOLELY THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR HAS A CONTRACTUAL OBLIGATION TO COMPLY WITH ALL APPLICABLE LAWS AND REGULATIONS INCLUDING THOSE OF OSHA. AT NO TIME WILL EITHER THE OWNER OR THE OWNER'S REPRESENTATIVE TAKE RESPONSIBILITY FOR EITHER THE MEANS OF THE WORK OR THE SAFETY OF THE CONTRACTOR'S EMPLOYEES.
2. LOCATION, SIZE AND DEPTH OF BURIED UTILITIES HAVE ARE NOT SHOWN ON THE PLANS. EXCAVATIONS HAVE NOT BEEN MADE TO VERIFY LOCATIONS OR DEPTH OF BURIED UTILITIES. THE EXACT LOCATION AND ELEVATION OF ALL UTILITIES MUST BE DETERMINED BY THE CONTRACTOR. IT SHALL BE THE DUTY OF THE CONTRACTOR TO ASCERTAIN WHETHER ANY FACILITIES MAY BE PRESENT. OVERHEAD UTILITIES AND UNDERGROUND SERVICES ARE NOT SHOWN. THE CONTRACTOR SHALL VERIFY THE ELEVATION OF POSSIBLE CONFLICTING UTILITIES PRIOR TO CONSTRUCTION. CONTRACTOR SHALL CONTACT "KANSAS ONE CALL SYSTEM, INC." (1-800-344-7233) A MINIMUM OF 2 FULL WORKING DAYS PRIOR TO THE START OF CONSTRUCTION FOR NOTIFICATION OF UTILITY OWNERS AND FIELD LOCATION OF ALL UTILITIES.
3. EXISTING FACILITIES (UTILITIES, PAVEMENTS, ETC) DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL EXPENSE TO THE OWNER.
4. ALL OPEN EXCAVATIONS SHALL BE PROTECTED WITH SAFETY FENCE.
5. CONTRACTOR IS REQUIRED TO MAINTAIN POSITIVE DRAINAGE ON THE SITE THROUGHOUT THE PROJECT DURATION.
6. CONSTRUCTION ACTIVITIES ARE TO BE LIMITED TO THE IMMEDIATE WORK SITES.
7. NO SOIL BORINGS HAVE BEEN PERFORMED FOR THIS PROJECT. THE CONTRACTOR SHALL BEAR ALL COSTS AND RESPONSIBILITY FOR ANY SOIL INVESTIGATIONS DEEMED NECESSARY FOR THE PREPARATION OF THE BID OR EXECUTION OF THE WORK.
8. THE CONTRACTOR SHALL APPLY NECESSARY MOISTURE TO THE CONSTRUCTION AREA AND TEMPORARY HAUL ROADS TO PREVENT THE SPREAD OF DUST.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY PINS, MONUMENTS, AND SECTION CORNERS. THE CONTRACTOR SHALL BE REQUIRED TO REESTABLISH ANY PROPERTY PINS, MONUMENTS AND SECTION CORNERS THAT ARE DAMAGED OR DESTROYED BY CONSTRUCTION OPERATIONS. SUCH ITEMS SHALL BE REESTABLISHED BY A LICENSED LAND SURVEYOR IN ACCORDANCE WITH STATE LAW.
10. ALL OBSTRUCTIONS ENCOUNTERED WITHIN THE LIMITS OF THE WORK SUCH AS FENCES, MAILBOXES, STREET MARKERS, SIGNS, ETC., SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR IN COMPLIANCE WITH ALL APPLICABLE FEDERAL, STATE AND LOCAL REGULATIONS. PAYMENT SHALL BE SUBSIDIARY TO OTHER ITEMS IN THE PROPOSAL UNLESS NOTED OTHERWISE.
11. BORROW AREAS PROVIDED BY THE CONTRACTOR SHALL BE APPROVED BY THE OWNER & ENGINEER AS TO THE SUITABILITY OF MATERIAL AND LOCATION. SPECIAL CARE SHALL BE TAKEN IN THIS APPROVAL TO MINIMIZE THE INCREASE OF SILTATION AND TURBIDITY OF STREAMS, LAKES AND RESERVOIRS AND TO AVOID INTERFERENCE WITH THE MOVEMENT OF MIGRATORY FISH. AREAS WHICH, IN THE OPINION OF THE ENGINEER, MAY LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE APPROVED.  
  
ALL BORROW AREAS SHALL BE SUBMITTED TO THE ENGINEER FOR COMMENT FROM THE KANSAS HISTORICAL SOCIETY AND THE KANSAS DEPARTMENT OF WILDLIFE AND PARKS PRIOR TO ANY EXCAVATION.  
  
IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RESTORE, SEED AND/OR COMPLETE OTHER OPERATIONS NOTED IN THE AGREEMENT WITH THE LANDOWNER, APPROVED BY THE ENGINEER, ON ALL DISTURBED AREAS USED TO PROVIDE BORROW AREAS FOR COMMON EXCAVATION (CONTRACTOR FURNISHED).

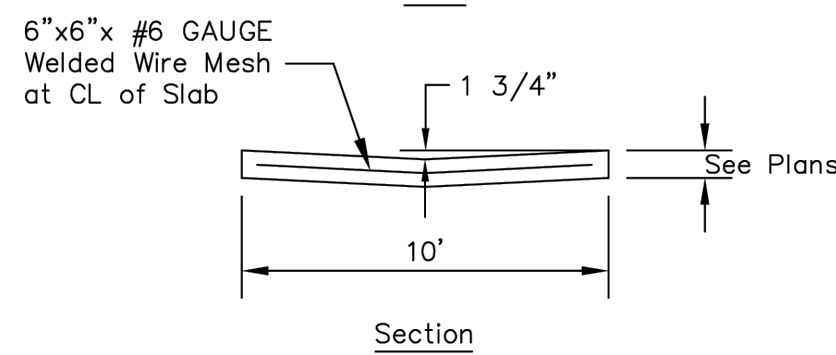
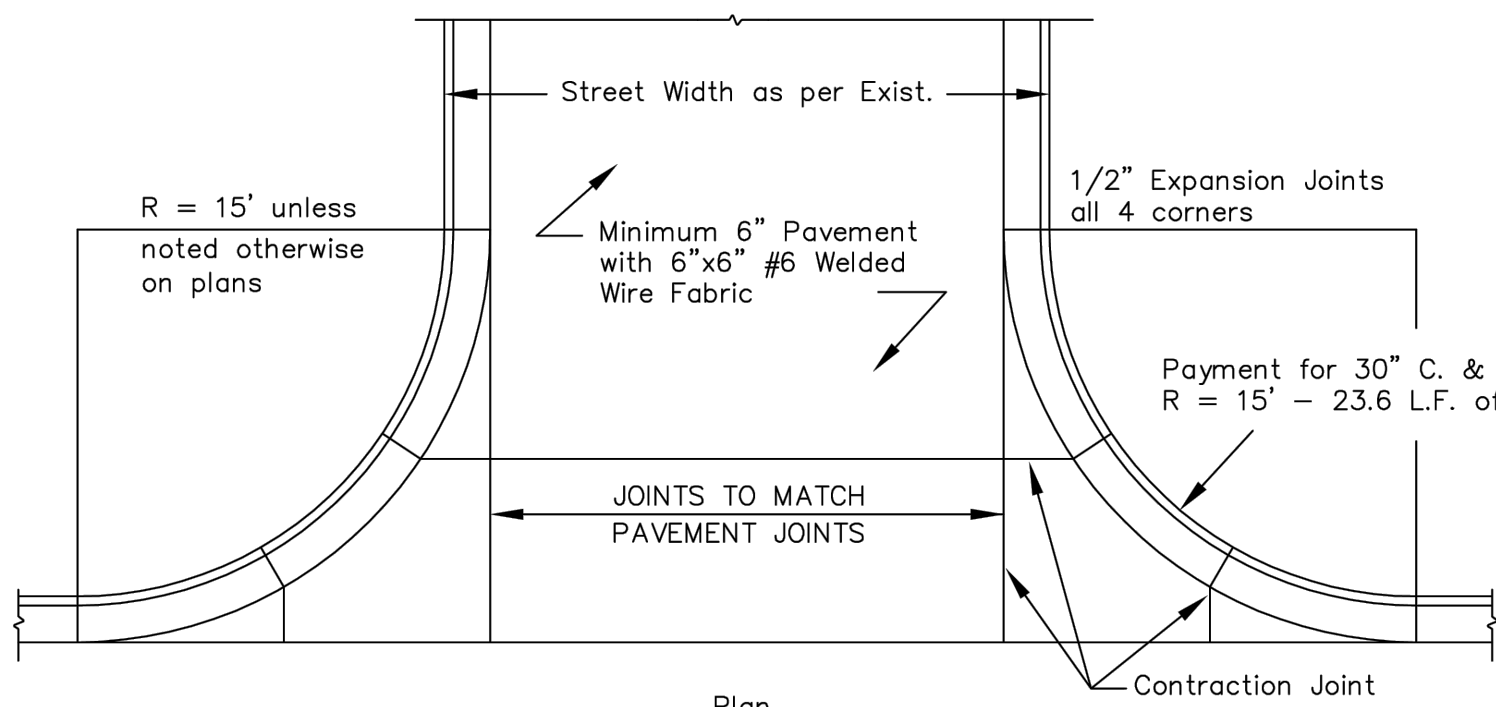
EXCAVATION REQUIRED FOR PLACING SELECT SOIL IS INCLUDED IN THE COMMON EXCAVATION QUANTITIES.

12. EXCAVATION SHOWN TO BE WASTED SHALL BE WASTED ON SITES PROVIDED BY THE CONTRACTOR. THESE SITES SHALL BE APPROVED BY THE OWNER & ENGINEER AS TO SUITABILITY, APPEARANCE, AND SITE LOCATION. LOCATIONS THAT, IN THE OPINION OF THE OWNER & ENGINEER, WILL LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE APPROVED.
13. ALL TREES, HEDGE ROWS, SHELTERBELTS, AND WOODY SHRUBS NOT SHOWN TO BE REMOVED AND LOCATED BETWEEN THE CONSTRUCTION LIMITS AND THE RIGHT OF WAY OR EASEMENT LINE SHALL BE SPARED UNLESS OTHERWISE DIRECTED BY THE ENGINEER TO BE REMOVED. ALL TREES & SHRUBS SHALL REMAIN.
14. TREES AND SHRUBS IN THE PUBLIC RIGHT OF WAY, WHICH ARE IN DIRECT CONFLICT WITH THE PROPOSED CONSTRUCTION, SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR WITH THE ENGINEER'S APPROVAL. TREES AND SHRUBS, WHICH ARE NOT IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION AND NOT SHOWN TO BE REMOVED, SHALL BE SAVED AND PROTECTED FROM DAMAGE.
15. A FULL DEPTH SAW CUT OF THE EXISTING TOTAL PAVEMENT THICKNESS SHALL BE PROVIDED AT LOCATIONS WHERE PROPOSED CONSTRUCTION ABUTS AN EXISTING SURFACE COURSE OR PAVEMENT FOR WHICH PARTIAL REMOVAL OF THE SURFACE OR PAVEMENT IS REQUIRED. REMOVAL LIMITS SHOWN WITHIN THREE (3) FEET OF EXISTING JOINTS SHALL BE EXTENDED TO THE EXISTING JOINT. SUCH SAW CUTS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE CONSIDERED AS SUBSIDIARY TO THE REMOVAL OF THE SURFACE OR PAVEMENT.
16. RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES, PAVEMENT REMOVAL AND EXCAVATION TO BE WASTED SHALL BE DISPOSED OF ON SITES PROVIDED BY THE CONTRACTOR. THESE SITES SHALL BE APPROVED BY THE OWNER & ENGINEER AS TO SUITABILITY, APPEARANCE AND SITE LOCATIONS. LOCATIONS THAT, IN THE OPINION OF THE OWNER & ENGINEER, LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE PERMITTED. EXCESS EXCAVATION TO BE WASTED SHALL BE SUBSIDIARY TO "EXCAVATION".
17. ALL DISPOSAL SITES MUST BE APPROVED BY THE KANSAS DEPARTMENT OF HEALTH AND ENVIRONMENT. MATERIAL EITHER STOCKPILED OR DISPOSED OF IN A FLOOD PLAIN WILL REQUIRE A KANSAS STATE BOARD OF AGRICULTURE PERMIT. ANY MATERIAL DUMPED IN WATERS OF THE UNITED STATES OR WETLANDS IS SUBJECT TO U.S. CORPS OF ENGINEERS PERMITTING REGULATIONS. ANY DISPOSAL ON CITY OF DODGE CITY AIRPORT PROPERTY IS SUBJECT TO FEDERAL AVIATION ADMINISTRATION (FAA) APPROVAL.
18. ANY MATERIAL BURIED OR STOCKPILED BEYOND THE APPROVED CONSTRUCTION LIMITS WILL REQUIRE ADDITIONAL ARCHEOLOGICAL INVESTIGATIONS UNLESS BURIED IN A PREVIOUSLY APPROVED BORROW LOCATION.
19. ALL SHALLOW STORM SEWER PIPE, LESS THAN 6-INCHES OF COVER FROM TOP OF PIPE TO BOTTOM OF PAVEMENT, PLACED BENEATH STREET OR DRIVEWAY PAVEMENT SHALL RECEIVE FLOWABLE FILL BACKFILL. THE CONTRACTOR SHALL PLACE FLOWABLE FILL BACKFILL AROUND AND 6" OVER STORM SEWER PIPE UNDER PAVEMENT. THE BACKFILL SHALL BE SUBSIDIARY TO OTHER ITEMS IN THE CONTRACT. FLOWABLE FILL BACKFILL SHALL NOT BE REQUIRED WITH BOX CULVERTS.
20. UNLESS OTHERWISE NOTED ON THE PLANS, EXISTING SIGNS SHALL BE SALVAGED AND REUSED. WHERE A SIGN IS NOT REUSABLE, IT SHALL BE DELIVERED BY THE CONTRACTOR TO CITY OF DODGE CITY PUBLIC WORKS DEPARTMENT. THIS WORK SHALL BE SUBSIDIARY TO OTHER ITEMS IN THE CONTRACT.
21. CONTRACTOR SHALL COORDINATE STAGING AND MATERIAL STORAGE LOCATION WITH THE OWNER. PH.: 620-227-9355.
22. THE PROJECT SHALL FOLLOW CITY OF DODGE CITY ENGINEERING STANDARDS AND SPECIFICATIONS. IN CASES WHERE THE CITY OF DODGE CITY ENGINEERING STANDARD SPECIFICATIONS ARE AMBIGUOUS OR DO NOT ADDRESS SPECIFIC ISSUES, THE PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH KANSAS DEPARTMENT OF TRANSPORTATION 2007 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE.
23. EXISTING DRAINAGE BASINS SHALL REMAIN AND SHALL BE PROTECTED TO PREVENT DAMAGE. IF DAMAGED THE CONTRACTOR IS RESPONSIBLE FOR THE REPLACING OF THE DRAINAGE BASINS AT THE CONTRACTOR'S EXPENSE.
24. CONTRACTOR WILL COORDINATE WITH TRASH COMPANY TO RELOCATE DUMPSTERS DURING CONSTRUCTION.
25. ALL PAVEMENT MARKINGS, SYMBOLS, & STRIPING SHALL CONFORM TO THE LATEST MUTCD.
26. ALL ASPHALT MILLINGS SHALL REMAIN AT THE DCCC. COORDINATE WITH OWNER ON LOCATION OF STOCKPILE.
27. PROJECT START DATE IS MID-MAY WITH A FINISH DATE OF AUGUST 1. COORDINATE WITH OWNER ON START DATE.
28. CONTRACTOR SHALL PROVIDE ACCESS FOR THE PUBLIC OFF OF 14TH AVENUE DURING CONSTRUCTION AND PART OF THE CAMPUS PARKING LOTS. CONTRACTOR SHALL COORDINATE WITH OWNER ON THE PHASING OF THE CONSTRUCTION AND ACCESS.

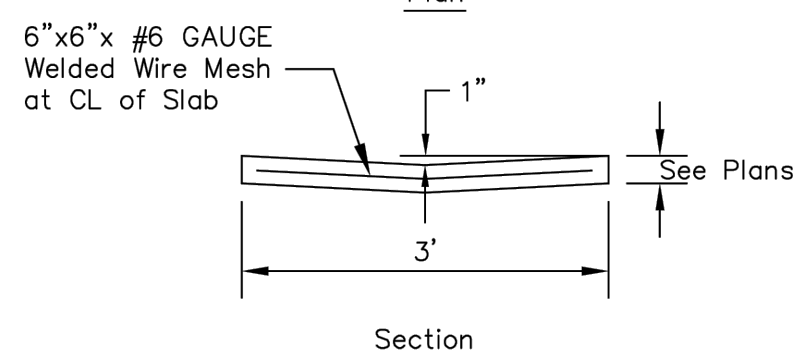
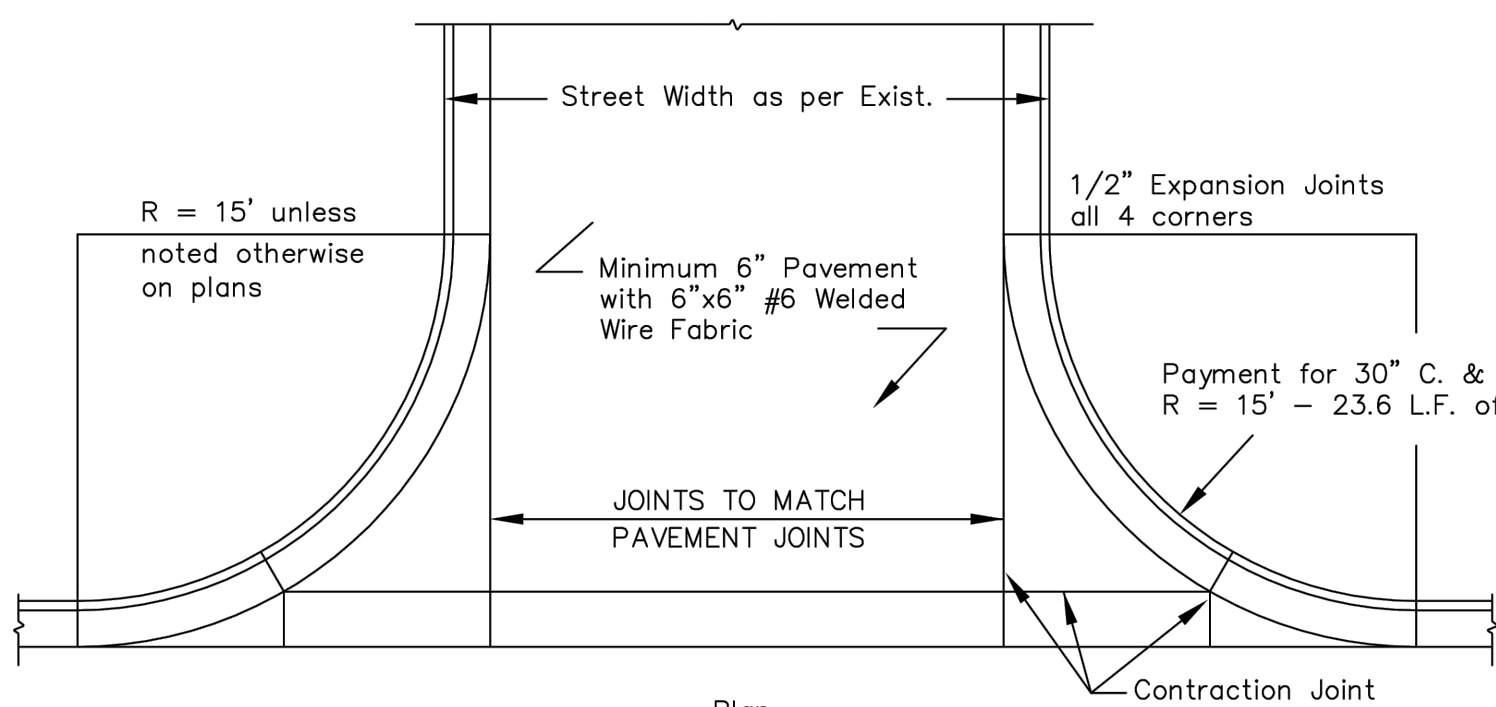


DODGE CITY COMMUNITY COLLEGE CONTACT

DIRECTOR OF FACILITIES & OPERATIONS: JARED STEVENS  
FACILITIES & OPERATIONS  
2510 N. 14TH AVE  
DODGE CITY, KS 67801  
(620) 225-8106



10' VALLEY GUTTER DETAILS

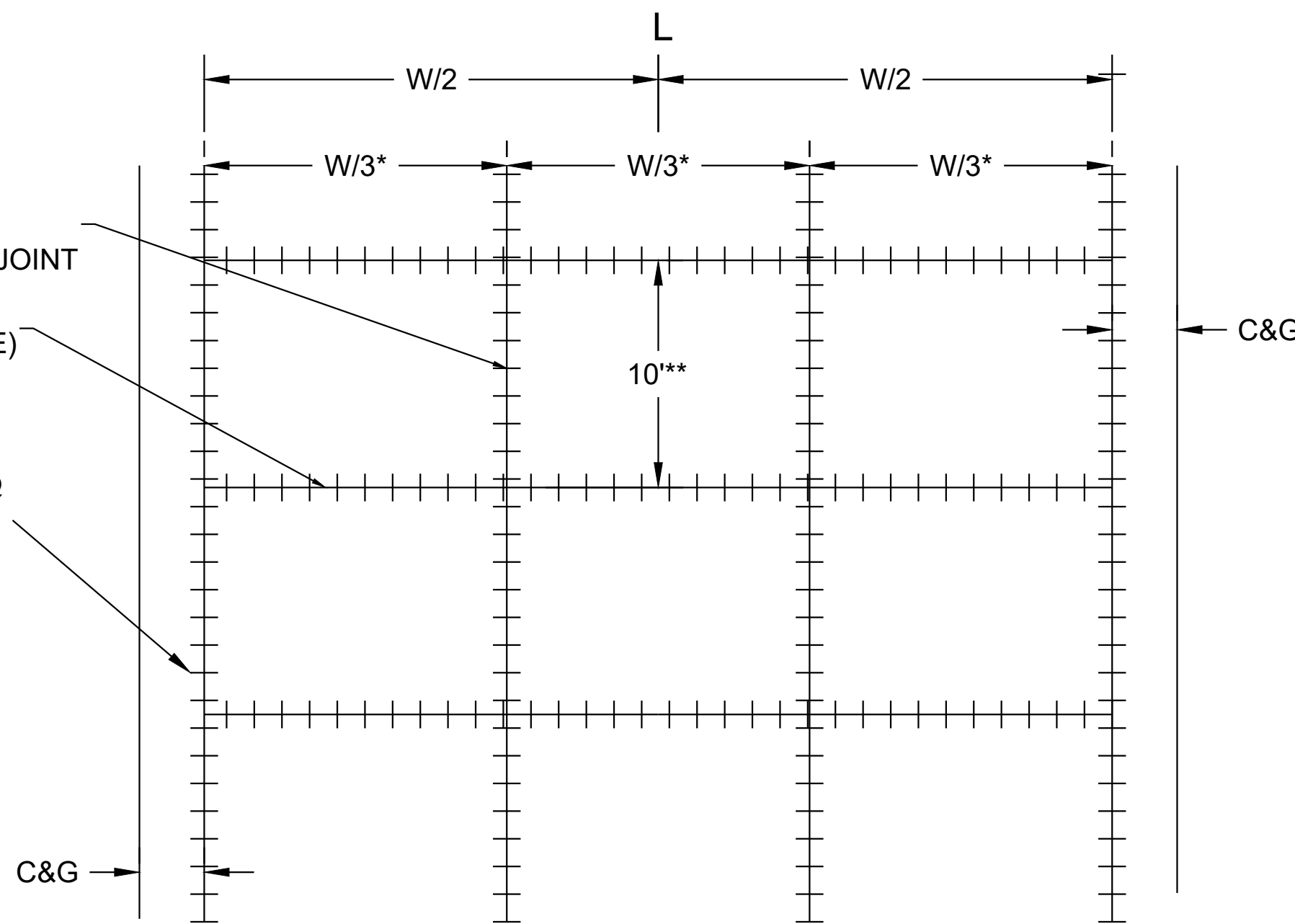


3' VALLEY GUTTER DETAILS

CONTRACTION JOINTS:  
~LONGITUDINAL JOINTS -  
SAW CUTS OR CONSTRUCTION JOINT  
~TRANSVERSE JOINTS -  
DOWEL JOINT (SEE TABLE)

| DOWEL SIZE |          |
|------------|----------|
| Pvmt Depth | Diameter |
| 7"         | 1"       |
| 8"         | 1"       |
| 9"         | 1 1/8"   |
| 10"        | 1 1/4"   |

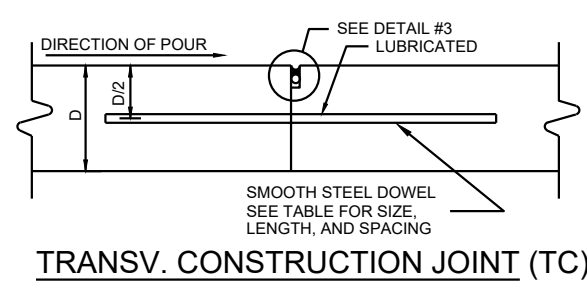
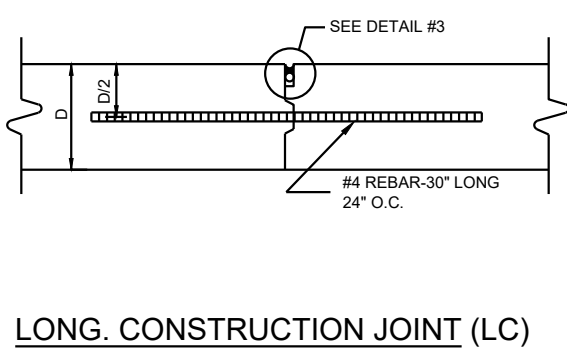
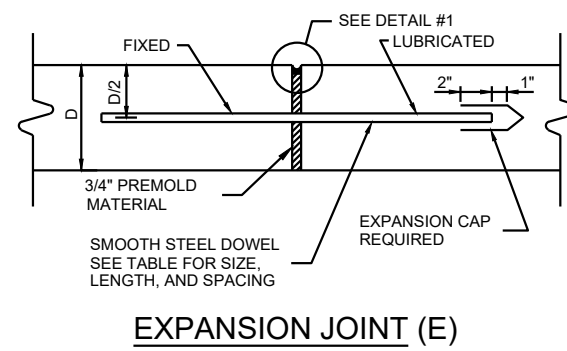
1/2" TIE BARS @  
2' o.c. (typ)



\* LONGITUDINAL JOINT SPACING MAY VARY AS DIRECTED BY THE ENGINEER.

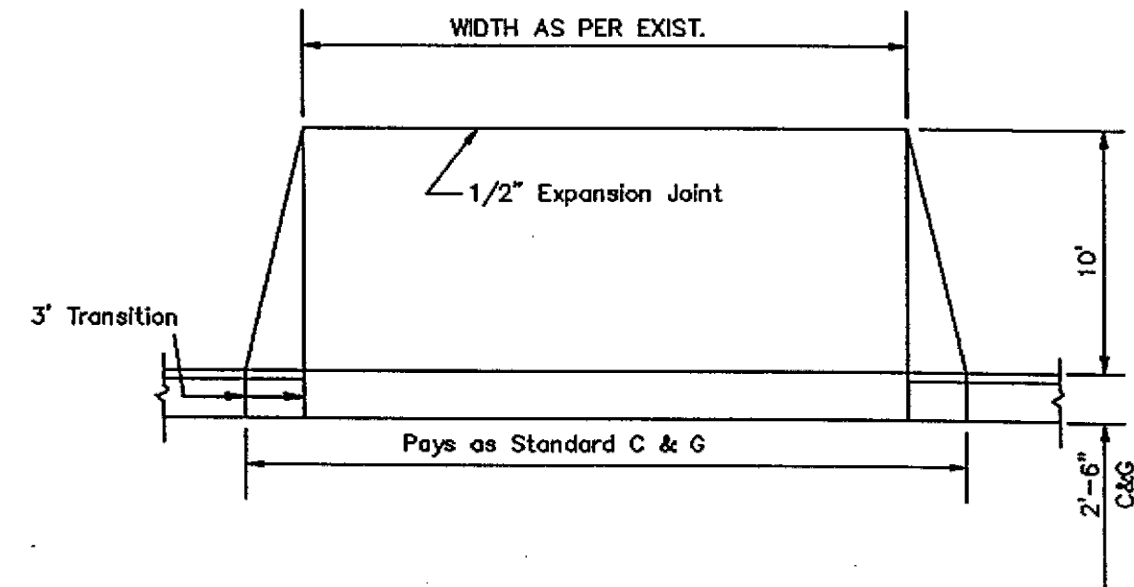
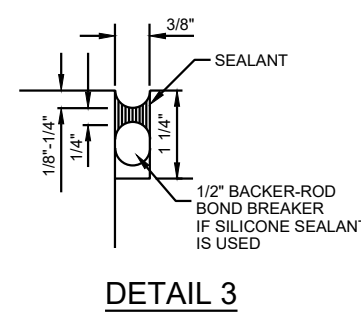
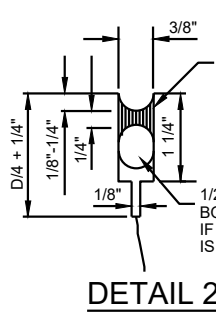
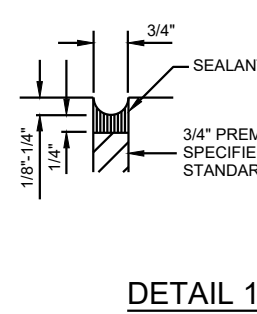
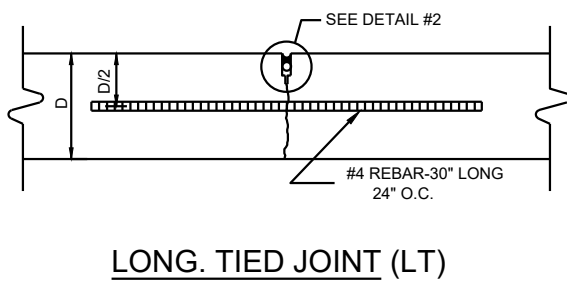
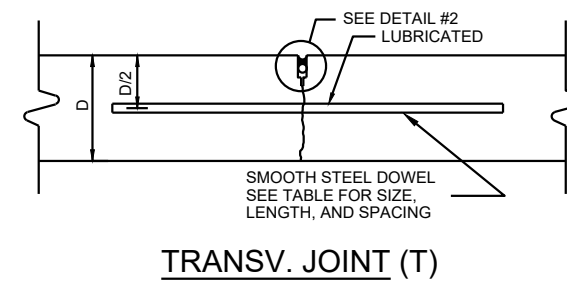
\*\* TRANSVERSE JOINT SPACING SHALL BE 10'.

### JOINT DETAILS

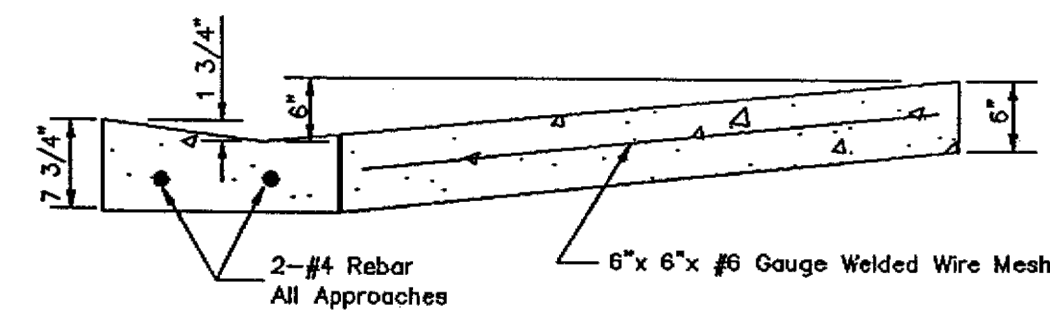


NOTE: ALL TRANSVERSE DOWELS SHALL BE SET ON BASKETS MADE FOR THAT PURPOSE. DOWELS SHALL BE WELDED TO BASKETS AT ALTERNATING ENDS. GREASE THE NON-WELDED ENDS. ALL DOWELS SHALL BE EPOXY COATED.

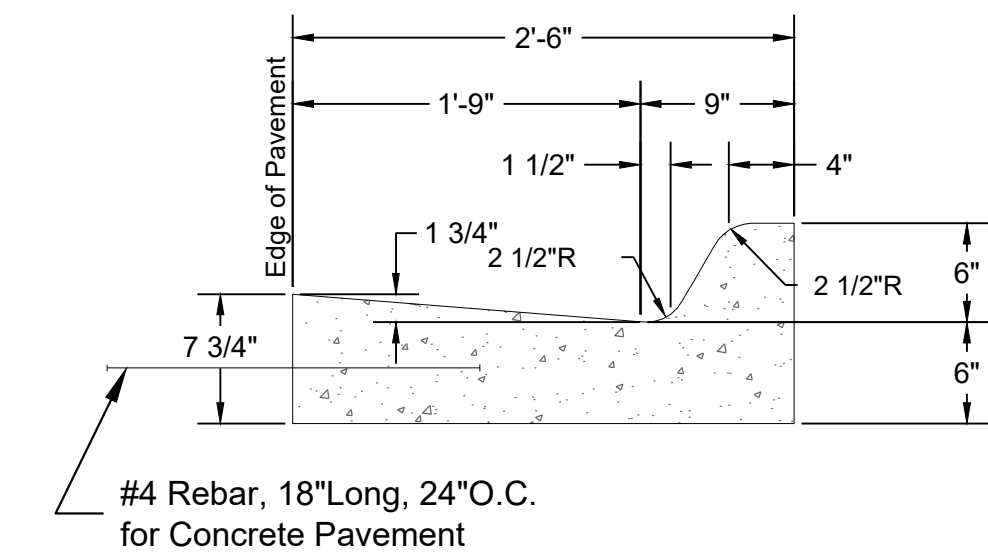
| PAVEMENT THICKNESS (in.) | DOWEL DIAMETER (in.) | DOWEL LENGTH (in.) | DOWEL SPACING (in.) |
|--------------------------|----------------------|--------------------|---------------------|
| 6                        | 3/4                  | 18                 | 12                  |
| 7                        | 3/4                  | 18                 | 12                  |
| 8                        | 1                    | 18                 | 12                  |
| 9                        | 1 1/8                | 18                 | 12                  |
| 10                       | 1 1/4                | 18                 | 12                  |
| 11                       | 1 3/8                | 18                 | 12                  |
| 12                       | 1 1/2                | 20                 | 12                  |



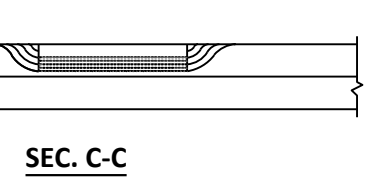
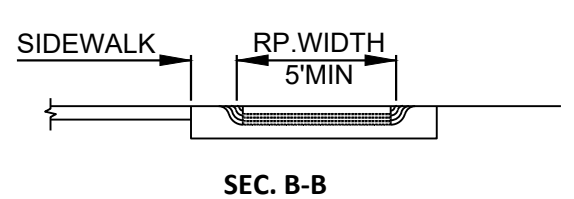
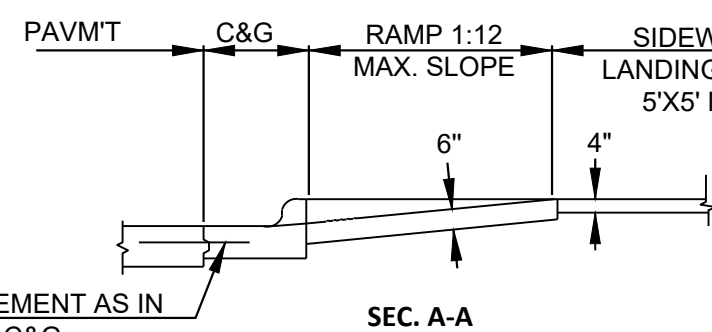
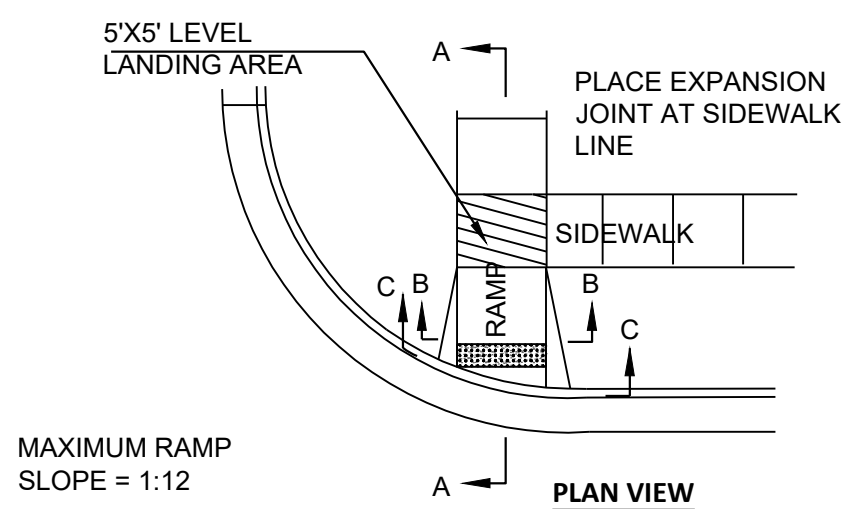
PLAN



SECTION

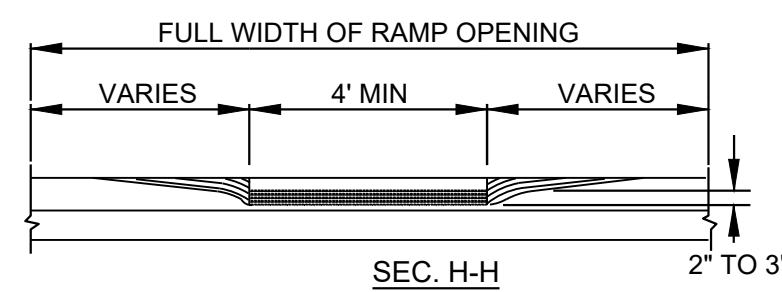
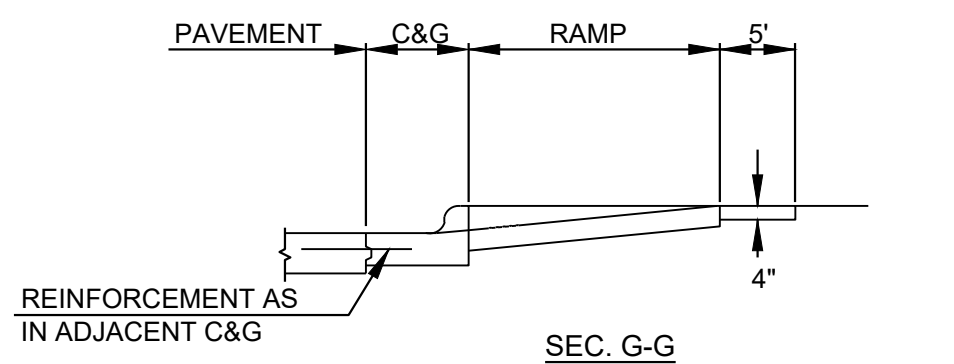
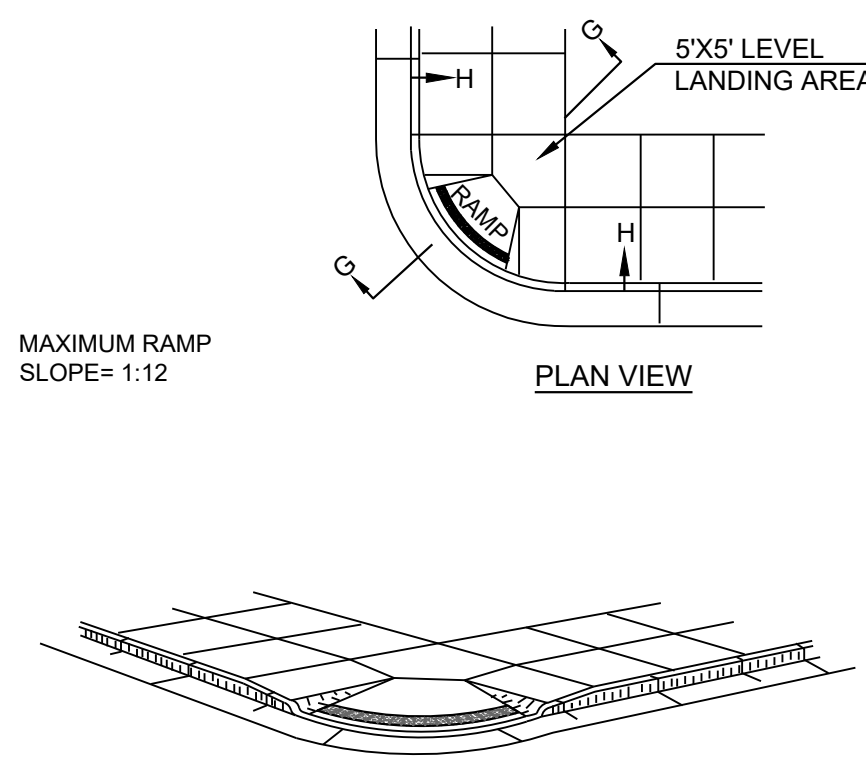


30" STANDARD CURB & GUTTER



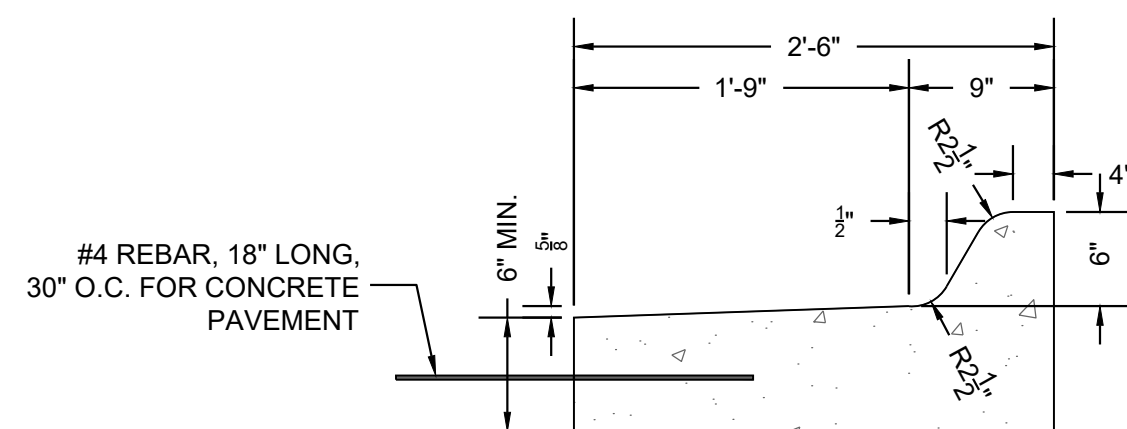
SIDEWALK RAMP TYPE 1  
(NOT TO SCALE)

- NOTE:**
- STANDARD RAMP IN RESIDENTIAL AREAS
  - 5' RAMP SHALL BE STANDARD FOR ALL NEW SIDEWALK CONSTRUCTION
  - AT THE DISCRETION OF THE CITY ENGINEER 4' RAMPS SHALL BE ALLOWED WHEN REPLACING EXISTING SIDEWALK

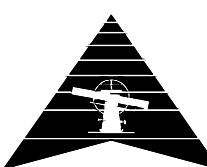


SIDEWALK RAMP TYPE 3  
(NOT TO SCALE)

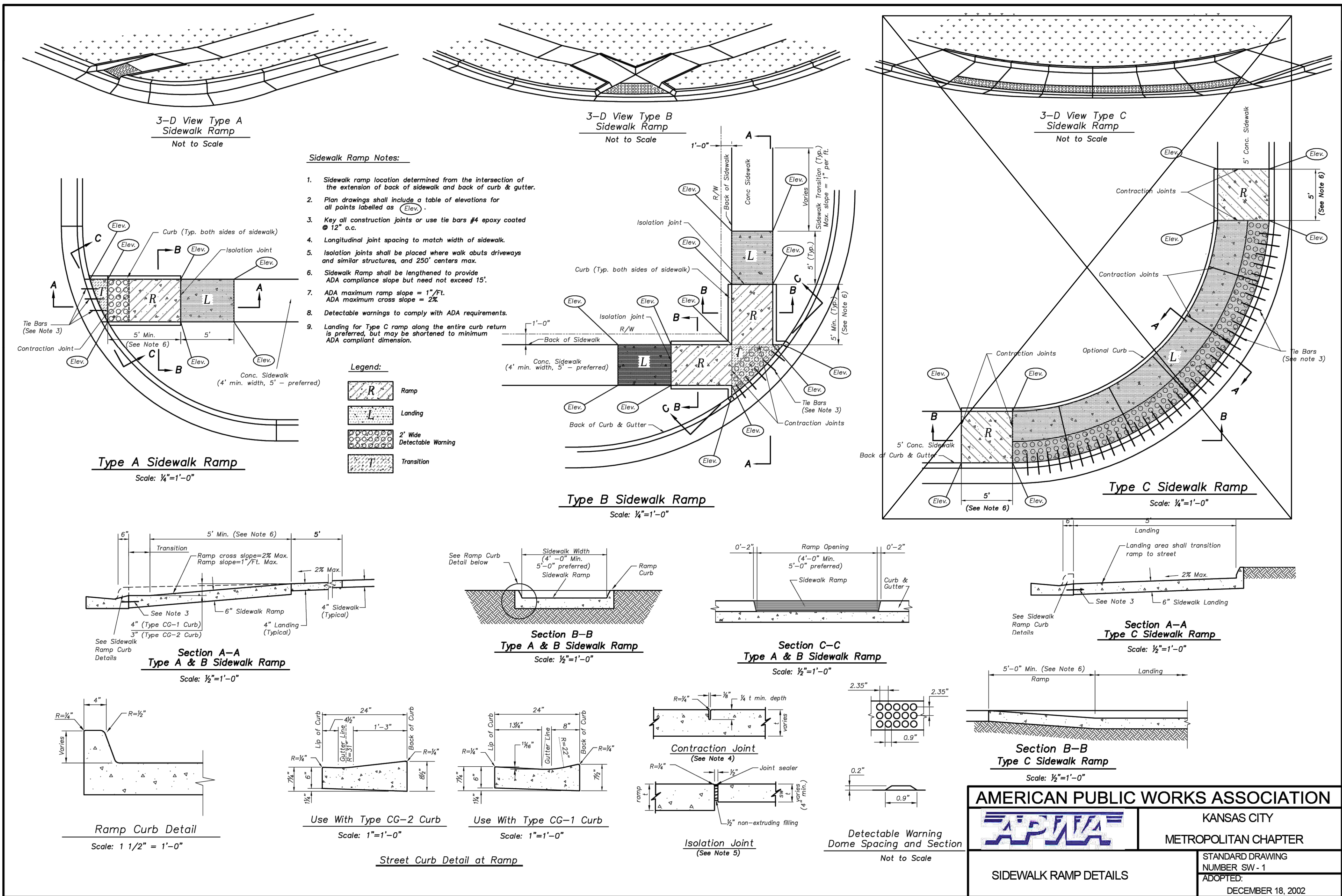
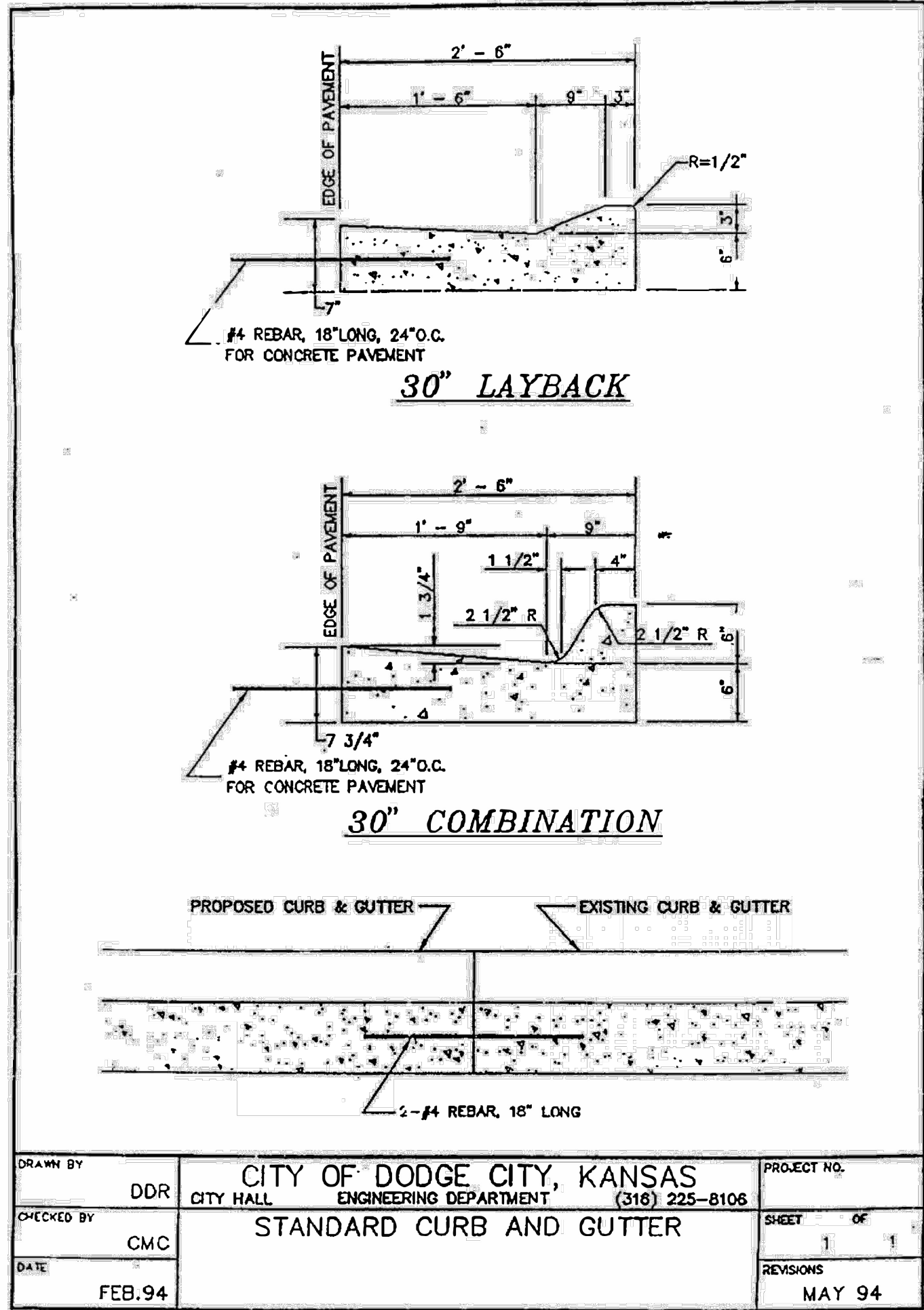
- NOTE:**
- TO BE USED WHERE SIDEWALK OCCUPIES ENTIRE AREA BETWEEN CURB AND PROPERTY LINE
  - LANDING AREA MAY BE PROVIDED EITHER SIDE 1:12 SLOPES ON FLARES MUST BE MAINTAINED.



30" REVERSE CURB & GUTTER SECTION







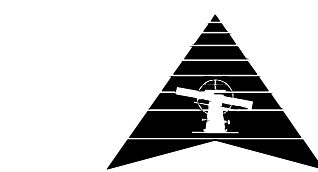


REVISION DESCRIPTION



REVISION DATE

NORTH



SCALE: NTS

PROJECT #: 2508-0324  
CHECKED BY: KML  
DRAWN BY: KML

DATE: 11/07/2025

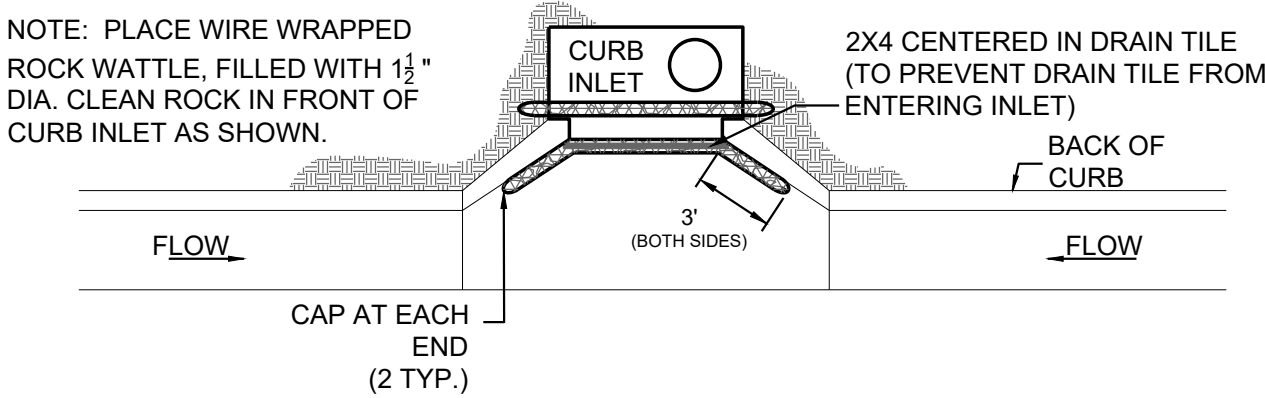
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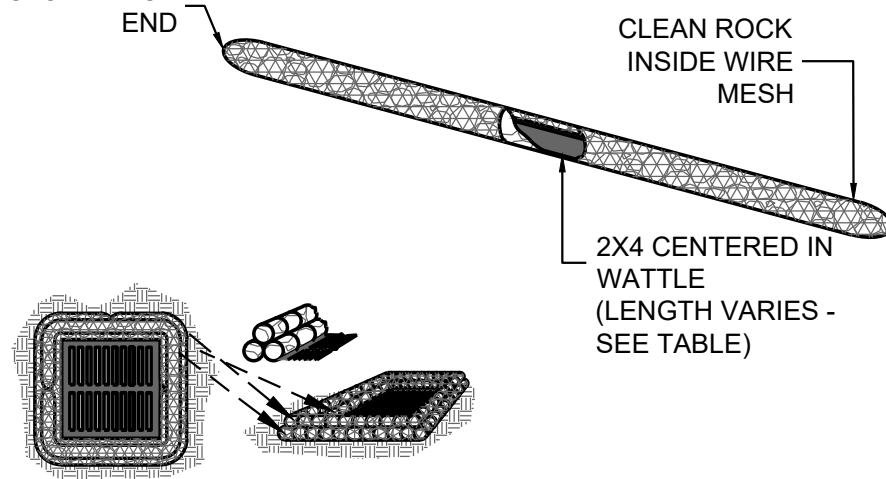
TOTAL SHEETS 5

WATTLE INLET PROTECTION  
WIRE WRAPPED CLEAN ROCK\*

CURB INLET PROTECTION



AREA INLET PROTECTION



NOTES FOR AREA INLET PROTECTION:

- SURROUND WIRE WRAPPED ROCK WATTLE, FILLED WITH 1½" DIA. CLEAN ROCK, AROUND AREA INLET AS SHOWN.
- INSURE EACH NEW WRAP DOES NOT BEGIN AND END IN THE SAME PLACE AS THE PREVIOUS.
- USE A MINIMUM OF THREE (3) WATTLES AROUND EACH INLET, STACKED AS SHOWN.

\* WHERE CLEAN ROCK IS NOT AVAILABLE, OTHER WATTLE MATERIAL MAY BE SUBSTITUTED. SUITABLE MATERIAL WILL NOT DETERIORATE OR DECOMPOSE DUE TO AGE OR WEATHER.

MATERIAL SPECIFICATION:

WATTLE INLET PROTECTION SHOULD BE CONSTRUCTED OF CLEAN ROCK THAT IS FREE OF FINES SUCH AS DUST OR SEDIMENT SMALLER THAN THE WIRE MESH ENCASING. THE STAKES USED TO ANCHOR WATTLES (WHERE POSSIBLE) SHALL BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 18" LONG. USE WIRE WRAPPED ROCK WATTLES FILLED WITH 1½" CRUSHED, CLEAN ROCK (RECYCLED CONCRETE OR SAND BAGS ARE NOT ACCEPTABLE). ORGANIC MATERIAL ENCASING AND/OR FILL SUCH AS COMPOST OR FIBER OF A VEGETATION ORIGIN IS PROHIBITED BECAUSE IT BIODEGRADES READILY.

PLACEMENT:

WATTLE INLET PROTECTION SHALL BE PLACED DIRECTLY AROUND THE OPENING OF A STORM INLET AND EXTEND A MINIMUM OF 2' ON EITHER SIDE OF THE OPENING. THE WATTLE(S) SHALL LAY DIRECTLY AGAINST THE CURB. TIMELY REMOVAL OF SEDIMENT MUST OCCUR FOR THE WATTLE TO OPERATE PROPERLY IN THIS LOCATION.

PROPER INSTALLATION METHOD:

- OVER PAVED SURFACES, THE LENGTH OF THE WATTLE PLACED IN FRONT OF THE INLET OPENING SHALL BE REINFORCED WITH A 2X4 CENTERED IN THE WATTLE.
- THE LENGTH OF 2X4 SHALL EXTEND 3" BEYOND BOTH SIDES OF THE INLET OPENING.
- THE WATTLE SHALL BE IN CONTINUOUS CONTACT WITH CURB AND ROAD SURFACES WHEN POSSIBLE.
- NO DAYLIGHT SHALL BE SEEN UNDER THE WATTLE.
- THE WATTLE COVERING THE AREA DIRECTLY IN FRONT OF THE INLET WILL NOT HAVE DIRECT CONTACT WITH THE CURB BUT WILL HAVE DIRECT CONTACT WITH THE ROAD SURFACE.
- IN INSTANCES WHERE FINISHED GROUND DIRECTS FLOWS OVER THE TOP OF THE INLET, A WIRE WRAPPED ROCK WATTLE SHALL BE PLACED ON TOP OF THE INLET FOR ADDITIONAL PROTECTION FROM TOP-SIDE FLOWS.
- IF WATTLES ARE USED SUCCESSIVELY, DO NOT OVERLAP THE ENDS ON TOP OF EACH OTHER.
- WATTLES SHALL BE INSTALLED AND MAINTAINED IN CONFORMANCE WITH MANUFACTURERS' SPECIFICATIONS TO MEET SITE CONDITIONS AND IN ACCORDANCE WITH GOOD ENGINEERING PRACTICES.

NOTE: THE INSTALLATION AND MAINTENANCE OF WATTLES SHALL NOT NEGATIVELY IMPACT TRAFFIC SAFETY.

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

- WATTLES SHALL BE PLACED DIRECTLY IN FRONT OF THE INLET OPENING. THIS ALLOWS OVERTOPPING WATER TO FLOW DIRECTLY INTO THE INLET INSTEAD OF ONTO NEARBY SOIL CAUSING SCOUR.
- WHEN MULTIPLE WATTLES ARE USED IN A CONTINUOUS ROW, THE ENDS SHALL OVERLAP HORIZONTALLY SO THAT NO DAYLIGHT CAN BE SEEN AT EACH OVERLAPPING POINT. THE UPHILL END OF THE OVERLAPPING WATTLE SHALL BE PLACED ON THE FLOW SIDE OF THE DOWNHILL END OF THE OVERLAPPED WATTLE.

INSPECTION AND MAINTENANCE:

WATTLE INLET PROTECTION SHALL BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF ½" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHALL BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW AROUND THE WATTLE?
- DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING WATTLES?
- ARE ANY WATTLES DISLODGED?
- ARE WATTLES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE WATTLE?